

ORDINANCE NO. 027/2012

AN ORDINANCE OF THE CITY OF MONROE, WASHINGTON, AMENDING THE NORTH KELSEY DESIGN GUIDELINES CONTAINED WITHIN THE NORTH KELSEY DEVELOPMENT PLAN TO REFLECT CURRENT AND ANTICIPATED DEVELOPMENT ACTIVITY, PROVIDING FOR SEVERABILITY; AND FIXING A TIME WHEN THE SAME SHALL BECOME EFFECTIVE.

WHEREAS, on August 6, 2003, the Monroe City Council adopted Ordinance No. 015/2003, an ordinance adopting the North Kelsey Development Plan including the North Kelsey Design Guidelines into the City of Monroe Comprehensive Plan; and

WHEREAS, on October 18, 2011, the Monroe City Council adopted Ordinance No. 026/2011, which amended Chapter 18.10 of the Monroe Municipal Code to require compliance with the North Kelsey Design Guidelines for all development within the North Kelsey Planning Area.; and

WHEREAS, on July 10, 2012, the Monroe City Council adopted Ordinance No. 017/2012, which amended Ordinances No. 15/2003 and No. 036/2008 to remove incorporation into the Land Use Element of the Comprehensive Plan and the adoption by reference of the North Kelsey Development Plan.; and

WHEREAS, the nation-wide economic downturn beginning in 2008 has compromised the viability and feasibility of the North Kelsey Development Plan as envisioned in 2003; and

WHEREAS, the Planning Commission held a public workshop on September 24, 2012, and a public hearing on October 15, 2012, and recommended approval of the proposed amendments to the North Kelsey Design Guidelines; and

WHEREAS, on October 10, 2012, the proposed development regulation amendments were submitted to the Washington State Department of Commerce for state agency for review; and

WHEREAS, on October 19, 2012, a State Environmental Protection Act Determination of Non-significance was issued;

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF MONROE, WASHINGTON, DO ORDAIN AS FOLLOWS:

Section 1. the Monroe City Council adopts the Findings and Conclusions contained in the staff report attached as Exhibit A and incorporated by this reference as if set forth in full.

Section 2. The North Kelsey Design Guidelines, referenced by MMC 18.10.135(B) are hereby replaced with the document attached as Exhibit B and incorporated by this reference as if set forth in full.

Section 3. Severability. If any section, sentence, clause or phrase of this ordinance should be held to be invalid or unconstitutional by a court of competent jurisdiction, such


invalidity or unconstitutionality shall not affect the validity or constitutionality of any other section, sentence, clause or phrase of this ordinance.

Section 4. Effective Date. This ordinance shall be in full force and effect five (5) days from and after its passage and approval and publication as required by law.

PASSED by the City Council and APPROVED by the Mayor of the City of Monroe, at a regular meeting held this 18th day of December, 2012.

1st Reading: 10/23/2012
2nd Reading: 12/18/2012
Published: 12/25/2012
Effective: 12/30/2012

CITY OF MONROE, WASHINGTON:



Robert G. Zimmerman, Mayor

ATTEST/AUTHENTICATED:



Eadye Martinson, Deputy City Clerk

APPROVED AS TO FORM:



J. Zachary Lell, City Attorney

EXHIBIT A
Findings and Conclusions

North Kelsey Development Plan Design Guidelines Update
City of Monroe ZCA2012-07
December 2012

The North Kelsey Development Plan Design Guidelines Update is analyzed under the procedures and criteria outlined below.

FINDINGS

Consistency with the City of Monroe 2005-2025 Comprehensive Plan

This proposed amendment is supported by existing goals and policies as listed below.

Land Use Goals

LUG1 - To pursue well-managed, orderly expansion of the City and actively influence the character of the City by managing land use change and by developing City regulations, facilities and services in a manner that directs and controls land use patterns and intensities.

LUG3 - Accommodate the city's expected growth in a way that enhances its character, quality of life and economic vitality.

LUG3 - Develop the North Kelsey Area into a vibrant, mixed use development that will provide a visible and accessible commercial center.

LUG8 - Provide for increased commercial development in the Monroe area that will enhance the character of major traffic corridors, downtown Monroe, and provide opportunities for neighborhood convenience shopping facilities within primarily residential areas.

LUP9 - Encourage commercial development designs that allow economically feasible development while at the same time providing attractive focal points in the community.

Economic Development Goals

EDG1 - Promote a strong, diversified, and sustainable local and regional economy, respecting the natural environment and preserving or enhancing the quality of life in the community.

EDG2 - Encourage economic development activities which take into consideration the capacities of the area's natural resources, public services, and facilities.

EDG3 - Promote the incorporation of transit, bicycle and pedestrian access in design of business, commercial and industrial developments.

EDG5 - Encourage and promote the development or enhancement of retail areas to achieve a vibrant shopping, dining and/or entertaining experience in the central business district.

2. Consistency with Chapter 20.04 MMC (SEPA)

An environmental checklist and supplemental non-project review sheet for environmental review was submitted on September 21, 2012. The SEPA Official issued a Determination of Non-significance on October 19, 2012. The comment/appeal period expired on November 9, 2012.

No comments or appeals were received.

3. Consistency with Chapter 36.70A (Growth Management Act)

The proposal addresses the following Planning Goals listed in RCW 36.70A.020:

- Urban Growth
- Reduce Sprawl
- Transportation
- Economic Development
- Property Rights
- Environment
- Citizen Participation
- Public Facilities and Services

Staff submitted the proposed amendment to the Department of Commerce on October 10, 2012 for the 60 day review period, and received a Letter of Acknowledgement on October 11, 2012. No agency comments were received.

4. Public Notice

The city provided notice through the City's webpage, posting at City Hall, and publication of hearing notices in the Monroe Monitor.

CONCLUSIONS

The proposed amendment is in keeping with the goals and policies of the Comprehensive Plan; is consistent with Chapter 20.04 MMC and Chapter 197-11 WAC; and has met Growth Management Act and noticing requirements.

Exhibit B

North Kelsey Development Plan Design Guidelines Update 2012

APPENDIX 1: Design Guidelines

December 6, 2012

Approved Ord. 008/2003

Amended Ord. 024/2007

Amended Ord. 000/2012

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CHAPTER 1:

Introduction

A. Purpose

These design guidelines are a critical regulatory tool in implementing the design-related goals and objectives for the North Kelsey Planning Area and the North Kelsey Planned Development Area.

B. Intent

These guidelines are directed to creating a development within the North Kelsey planning area that:

- Provides a visible and accessible commercial district for the City of Monroe.
- Enhances downtown circulation for pedestrians and vehicles.
- Connects and integrates other downtown activities.
- Accommodates retail, office, and service development of various sizes and character as is appropriate for Monroe's small town character with design quality of the highest caliber.
- Enhances the town's identity as a regional attraction.

C. Application of Design Guidelines

The design guidelines apply to all new construction in the North Kelsey Planning Area (Figure 1). The sole exception to the guidelines involves interior remodels. The guidelines are intended to supplement other requirements in the Monroe Municipal Code. Where the guidelines and zoning ordinance standards conflict, the City shall determine which regulation applies.

All properties that are outside the development area, but within the North Kelsey Planning Area are subject to Chapters 5 through 7 of these Design Guidelines. This includes the privately owned parcels within the outlined area.

The specific planned development area is comprised of properties as reflected in Figure 1, Applicable Properties.

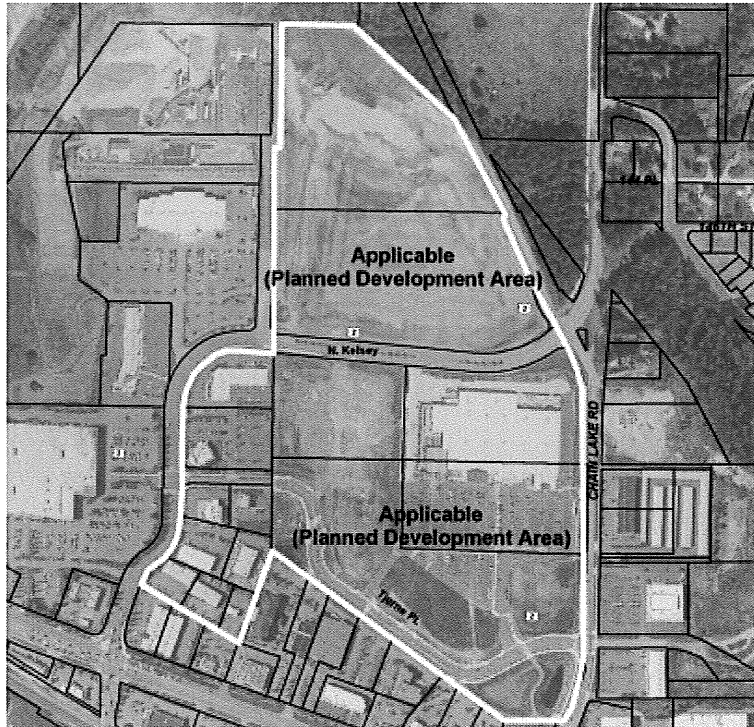


Figure 1: Applicable Properties.

D. Interpreting the Design Guidelines

The City retains full authority to determine whether or not a proposal meets these guidelines. Within the guidelines, certain words are used to indicate the relative importance and priority the City places upon the particular guideline. The words “shall,” “must,” and “is/are required” mean that the development proposal must comply with the guideline unless the City finds that:

- The guideline or requirement is not applicable or appropriate in the particular instance, or;
- The development proposal meets the intent of the guidelines in some other manner.

The words “is/are encouraged” mean that the action or characteristic is allowed and should be viewed as a positive element in the City’s review.

The project proponent may submit proposals that he/she feels meet the intent of the guidelines but not necessarily the specifics of one or more guidelines. In this case, the City will determine if the intent of the guideline has been met.

E. Review Process

The Design Guidelines will be used, along with other City ordinances and regulations, for the City’s review of development proposals within the North Kelsey Development Area.

CHAPTER 2: Site Configuration

The development must address the following principles: The proposal shall indicate how the development will connect to adjacent properties/parcels. The means of pedestrian and vehicular circulation, as well as building and entry orientation, must be as approved by the City in accordance with the Design Guidelines as applied to the entire area of applicability as stated in Chapter 1C.

1. Connects the following activities with an integrated pedestrian network:
 - Southeast: Chain Lake Road sidewalk.
 - North: North Kelsey Street.
 - West: North Kelsey Street near the entry to the Fred Meyer store.
 - South: Current and future development along Tjerne Place.

Gateway features and safe walking connections must be provided at these points.

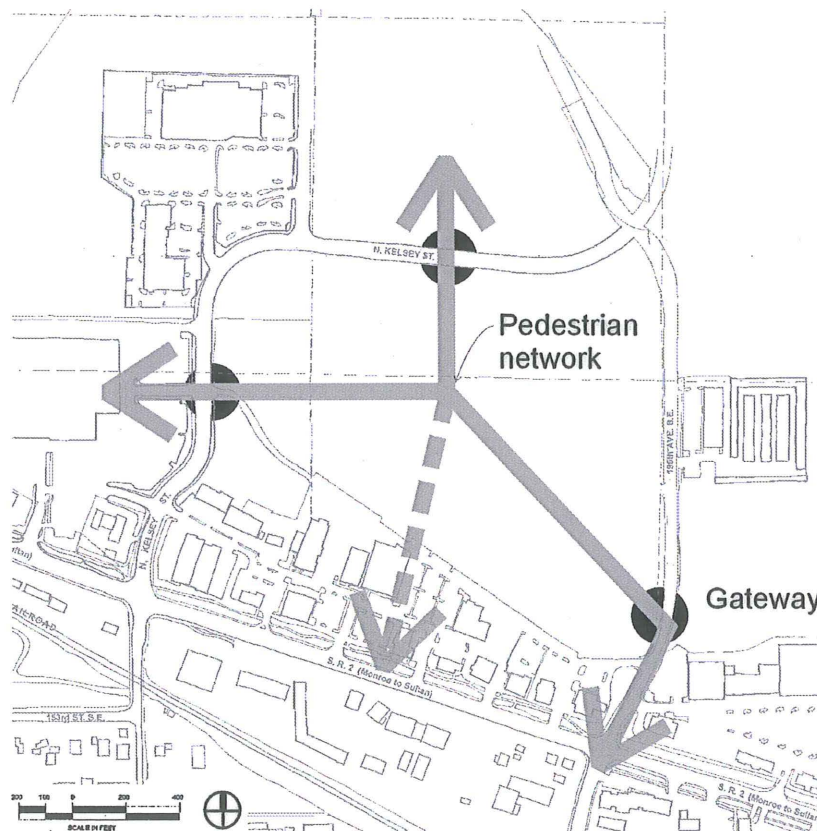


Figure 2: Key pedestrian connections.

2. Locates and treats large buildings to reduce their perceived scale to fit with neighboring structures and present an inviting, human scaled, pedestrian oriented character to the public. (See Architectural/Building Design guidelines, Chapter 5.)
3. Includes Tjerne Road to connect North Kelsey Street and Chain Lake Road, creating a loop system around the south parcel. Parking for future development should be accessed from this loop system (See Parking Area guidelines in Chapter 3 and Circulation Guidelines, Chapter 4). Opportunities for sharing access and parking between current and future uses, including the theater complex, should be explored.

The project proponent must demonstrate that the overall site layout and circulation system accomplishes these goals to the City's satisfaction.

CHAPTER 3: Site Planning

A. Public Open Space

Intent:

- Provide an attractive pedestrian environment.
- Provide outdoor spaces with pedestrian amenities.

Pedestrian-Areas

Guidelines:

1. Pedestrian-areas are encouraged along the pedestrian connections and near key building entries. They can be small to large widening of walking space, landscaped areas, areas for outdoor dining, or small play areas.
2. Pedestrian amenities shall be provided such as seating, plants, drinking fountains, distinctive paving, artwork, and such focal points as sculpture or water feature, should be provided.
3. Lighting fixtures should be approximately 10-15 feet above the surface and may be building mounted. The overall lighting in pedestrian areas should be at least 2 foot-candles, without any “dark spots” that could cause security problems. Ambient light from under canopies or storefronts may be included in the lighting calculations.
4. The spaces must have visual and pedestrian access (including barrier-free access) to abutting structures and public streets or pathways.
5. Landscaping that does not act as a visual barrier is encouraged
6. Buildings abutting pedestrian-oriented space must have pedestrian-oriented facades (see Building Orientation guidelines below).
7. See Chapter 6 Landscape Design for applicable landscaping guidelines.

B. Building Orientation

Intent:

- Provide an attractive pedestrian environment.
- Enhance the character of the streetscapes within and surrounding the area.
- Provide attractive building facades adjacent to parking lots.

General:

Buildings and ground floor businesses fronting pedestrian areas should be oriented towards these areas. Since these and other buildings also front onto parking lots or streets, buildings and businesses are strongly encouraged to provide secondary building entrances. These are referred to as “Secondary Pedestrian Facades.” Side and rear walls of new buildings in the planning area will also be visible and therefore must be designed and/or screened to provide an attractive streetscape. These are referred to as “Side or Rear Facades.” Locations, guidelines and standards for, Secondary Pedestrian Facades, and Side or Rear Facades are detailed below.

1. Secondary Pedestrian Façade:

- a. Where: Building facades adjacent to parking lots, pedestrian pathways, or streets, as designated by the City.
- b. Weather protection over the building entry and covering at least 50 percent of the overall façade is required.
- c. Storefront windows over at least 50 percent of the facades on the ground floor between the heights of 2 to 8 feet above the ground are required.
- d. Building entries along secondary pedestrian facades should utilize pedestrian-oriented lighting and/or decorative façade details. (See Building Elements and Details, Chapter 5).
- e. Blank walls must be treated in one or more of the following ways:
 - Planters or trellises with vines.
 - Landscaping that covers 30 percent of wall area within three years of planting.
 - Special materials (e.g., decorative patterned masonry).
 - Display windows.
 - Other treatment approved by the City.

2. Side and Rear Facades:

- a. Building facades building/business entries, or highly visible locations.
- b. What: While pedestrian building/business entries are not required here, they may be encouraged depending on specific site characteristics. Service elements may be located here (see Building Equipment and Service Area guidelines, Chapter 5). Facades shall be treated in two or more of the following ways (Figure 4):
 - Planters or trellises with vines.
 - Landscaping that covers 30 percent of wall area within three years of planting.
 - Special materials (e.g., decorative patterned masonry).
 - Display windows.
 - Other treatment approved by the City.
- c. Designated side and rear facades located along public streets should be set back at least 20 feet from the sidewalk to accommodate stormwater treatment methods and heavy landscaping.
- d. Visible building façades should be articulated per “Vertical Articulation” guidelines in Chapter 5 under Human/Pedestrian Scale.
- e. Creative use of building materials such as concrete and concrete masonry units is encouraged.

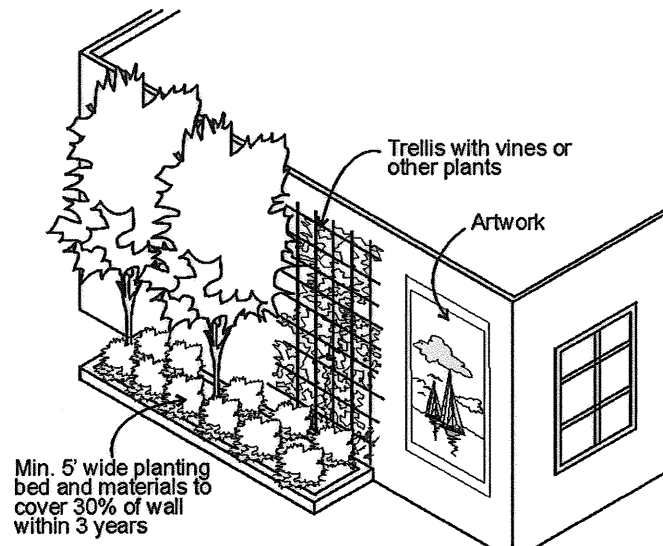


Figure 3: Blank wall treatments.

C. Parking Areas

Intent:

- To provide convenient parking areas that encourage people to leave their cars and walk throughout the North Kelsey Planning Area.
- To provide more flexibility in the design of the development by relaxing existing City parking standards.
- To provide parking areas that do not diminish pedestrian and visual qualities of the site.
- To maintain the built street edge through effective screening of all parking lots.
- To minimize the impacts of driveways.

Guidelines:

1. Parking areas shall conform to the requirement of MMC, Chapter 18.86 unless otherwise noted in these guidelines. This encompasses dimensional requirements, design, access, loading areas, number of parking spaces, parking area landscaping, and other parking-related requirements.
2. Parking requirements for retail uses shall be relaxed to 1 space per 250 square feet of gross floor area. The City may consider special provisions for joint use

of parking when two activities are less likely to occur simultaneously (e.g. office uses and entertainment facilities).

3. The landscaped buffer between the sidewalk and the parking area along Chain Lake Road must be expanded to at least 10 feet in width using either Type II or Type III Landscaping standards (MMC, Section 18.78.030) subject to City approval.
4. Pathways through parking lots should be provided. Pathways and crosswalks should be provided along every fourth parking isle or at intervals of less than 150 feet (See Figure 5). Pathways through parking areas should be separated from vehicle parking and travel lanes by use of contrasting surface materials, which may be raised above the level of the vehicular surface. Parking area pathways should be at least 4 feet in width.

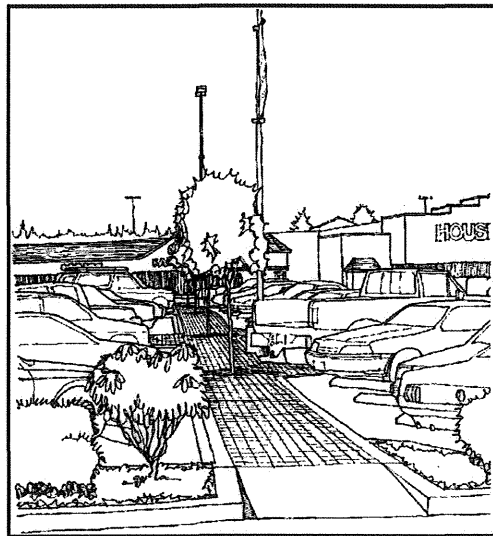


Figure 4: Parking pathway example.

5. Structured parking is encouraged provided the building meets the guidelines of Chapter 5.

D. Street Corners/Highly Visible Locations

Intent:

- To enhance the appearance of highly visible locations.
- To enhance the pedestrian environment.
- To establish a design identity for the North Kelsey Planning Area.

Guidelines:

The guidelines below highlight desirable design treatments (options noted below) for six specific street corners and/or highly visible locations. All proposals for sites should include at least one of the design treatments described below. EXCEPTION: Applicants may propose other design treatments for these sites if they can demonstrate successfully that the proposed treatment meets the intent of the guidelines.

1. Street Corner/Highly Visible Location Design Treatment Methods (also refer to Pedestrian Orientation guidelines, Chapter 3):
 - a. Locate a building towards the street corner (within 15 feet of corner property line). Building facades located here are encouraged to include a special element, such as a raised roofline, towers, or an extended parapet, along the most visible views of the structure (See Figure 6).
 - b. Provide a pedestrian walkway and/or plaza space at the corner leading directly to a building entry or shopping plaza space. May be appropriate in conjunction with a Monument Site Entry Sign.
 - c. Install substantial landscaping (at least 200 square feet of ground surface area with trees, shrubs, and or ground cover. May be appropriate in conjunction with a Monument Site Entry Sign.



Figure 5: Street corner example: This building celebrates its corner location by including a corner entry, pedestrian space, weather protection, parapet, and special signage.

2. Specific Sites (See Figure 7):

- a. SR-2/Chain Lake Road (northwest corner): Method “c” is the first preference. High priority site for a Monument Site Entry Sign.
- b. Chain Lake Road/Tjerne Place (both westerly corners): Method “a” or “b” is preferred for the northwest corner; Method “a” is preferred for the southwest corner, with Method “c” as a second preference.
- c. Chain Lake Road/North Kelsey Street (both westerly corners): Method “c” is preferred for both the northwest and southwest corners. High priority site for a Monument Site Entry Sign (see Signage guidelines, Chapter 7).
- d. North Kelsey Street (at key pedestrian crossing): Method “a” is preferred for all four corners.
- e. North Kelsey Street/Tjerne Place (easterly corners): Method “a” is the preferred treatment of both corners; Methods “b” and “c” are acceptable.

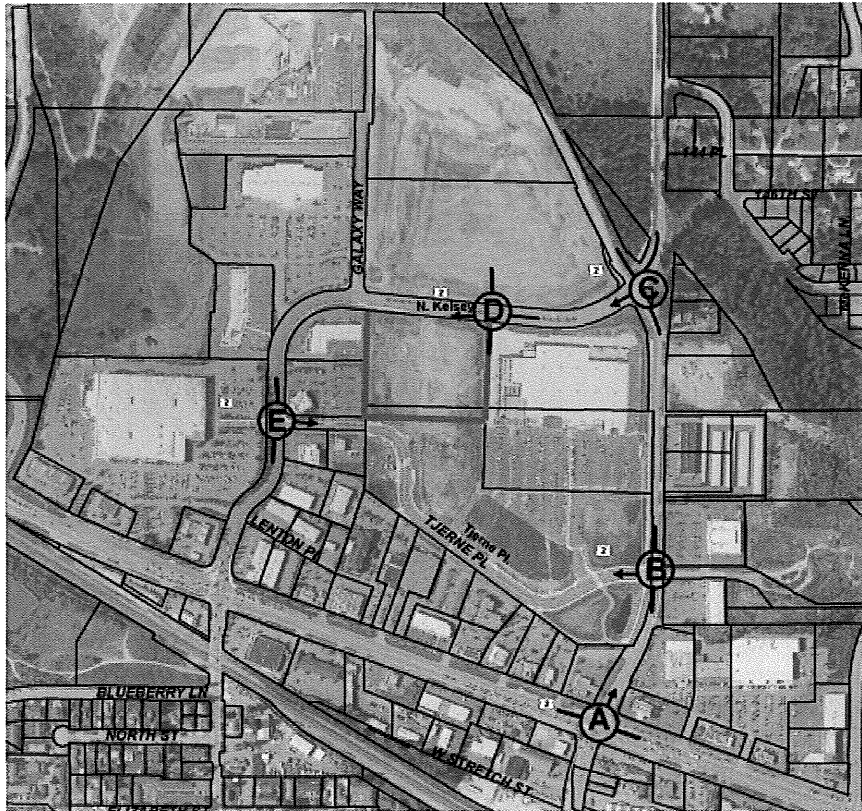


Figure 6: Highly visible locations.

CHAPTER 4:

Circulation

A. Sidewalks and Pathways

NOTE: Sidewalks refer to concrete pedestrian routes adjacent to public rights-of-way. Pathways refer to all other pedestrian routes.

Intent:

- Provide a safe environment for pedestrians to move throughout the North Kelsey planning area and separation of pedestrian and vehicular traffic.
- Create a varied and rich environment to encourage people to explore the area on foot.

Guidelines:

1. All public open spaces, walkways, and sidewalks shall meet ADA standards.
2. Sidewalks should be separated from the roadway by planting strips with street trees wherever possible. Planting strips should generally be at least 5 feet in width and include evergreen shrubs no more than 4 feet in height and/or ground cover in accordance with the City of Monroe Landscape Standards (MMC Chapter 18.78), and canopy-type broadleaf trees placed an average of 25 feet on center. EXCEPTIONS: Where space is limited, planting strips less than 5 feet in width may be permitted by the City; Street trees placed in tree grates may be more desirable than planting strips in key pedestrian areas.
3. Acceptable sidewalk widths may range from 4 to 12 feet depending on adjacent uses and anticipated pedestrian activity. Sidewalks along major connector routes such as North Kelsey Street or Chain Lake Road should be at least 8 feet in width to accommodate two couples passing each other.
4. Pedestrian crosswalks shall be provided at all intersections. These shall be indicated with distinctive paving.
5. The addition of texture to the ground plane of key sidewalks and pathways with unit pavers, bricks, tiles, or public artwork is encouraged.
6. Pathways that provide key access to other key sites are termed “Primary Pathways.” Primary pathway surfaces should be at least 15 feet in width to accommodate fire apparatus access and groups of people.
7. Other pathways are termed “Secondary Pathways.” Secondary Pathways may vary in width according to intended function and expected use (subject to City

8. approval). Where secondary pathways are located within corridors between structures, such corridors should be at least 12 feet in width.
9. Pedestrian amenities, including landscaping and seasonal flowers, benches, lighting, and/or artwork, shall be provided along Secondary Pathways to create visual interest (see Landscaping guidelines in Chapter 6).
10. Safe pathways to all uses and buildings and around and through parking areas are required (see Parking Area guidelines, Chapter 3).

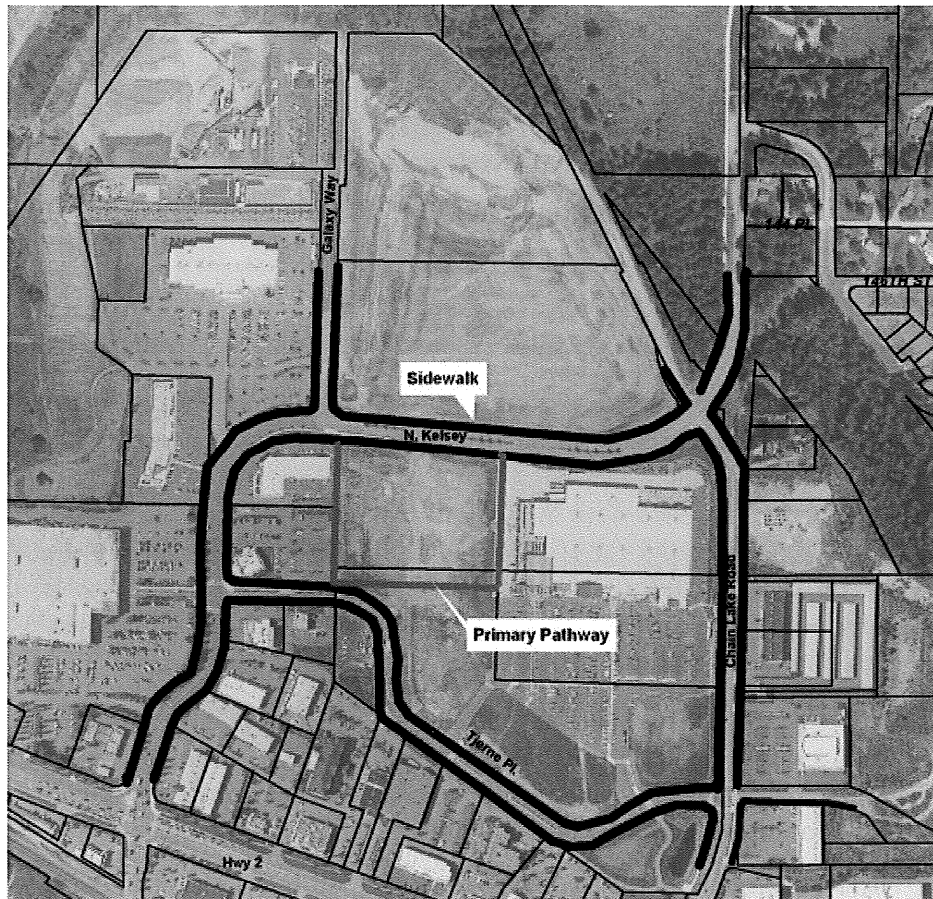


Figure 7: Example pedestrian network for the North Kelsey Planned Development Area.

B. Bicycle Circulation and Amenities

Intent:

- Provide safe and efficient bicycle access to and within the North Kelsey Planning Area.
- Promote bicycling as an alternative method of transportation.

Guidelines:

1. Safe bicycle access should be provided within each public right-of-way developed within the North Kelsey planning area. The City will consider the following options:
 - a. Bike Lanes. Standard bike lanes are 5 feet in width. This is the preferred option for Chain Lake Road (where there is sufficient right-of-way width) since bicycle lanes would connect with planned bicycle lanes north of the site.
 - b. Wide Curb Lanes. This involves 14-foot travel lanes rather than the standard 11- or 12-foot lanes so cyclists can safely share the road with vehicles. Although such wide curb lanes are often striped, they are not signed or officially designated as bike lanes. With limited space, this is often the most effective way to provide safe bicycle access.
 - c. Multi-Use Pathway. This combines bicycle and pedestrian access on an asphalt pathway separated from the roadway. Ideally, such a multi-purpose pathway should be 12 to 14 feet in width. Where space and use are expected to be limited, an 8-foot wide pathway (with center striping) may be acceptable. Pathway design should ensure adequate site distance.
2. Special care should be exercised on how either of these bicycle facilities transition to existing and planned off-site roadways – particularly Chain Lake Road and North Kelsey Street towards SR-2. Where necessary, provide signage to note safest bicycle access routes.

CHAPTER 5:

Architectural/Building Design

A. Architectural Concept

Intent:

- Create, through the architectural, landscape, open space, and gateway elements, an identity that reflects Monroe’s vernacular architectural character.
- Provide a high-quality image with well-designed and detailed buildings, minimization of corporate identity elements (stock buildings and signs), and an emphasis on subtlety and refinement rather than on flashy or trendy design themes.
- Create varied, non-homogenous buildings within the planned development area that give the sense of natural evolution over time rather than a result of a single, one-step development—and to emphasize the fact that the building elements can naturally evolve and change over time.

Guidelines:

1. The buildings proposed for the North Kelsey development area should be based on a comprehensive architectural concept that achieves the intent statements above. Specifically, the design of the specific buildings should address:
 - Pedestrian interest, comfort and connections throughout the area.
 - Building size and massing elements at human scale.
 - Perceived massing of the building relative to nearby structures, pedestrian areas, and landscape elements.
 - Monroe’s architectural and cultural setting.
 - The variety of sequential experiences and design characters within the site.

While the individual design guidelines in this section address some of these issues specifically, the intent of this guideline is to encourage the designers to consider how the various aspects of the design work together. Applicants should be prepared to demonstrate how the proposed buildings respond to the intent statements. The City will review applicants’ proposals and determine whether or not they meet the intent.

B. Human/Pedestrian Scale

Intent:

- Create a development with human scaled appearance and informal architectural character.
- To architecturally address large buildings to ensure that they do not dominate.

- Provide interesting and sheltering pedestrian-oriented facades.

Guidelines

1. **Building Height:** Commercial/office buildings should be 1 to 3 stories high, with a maximum height of 35 feet. The City will consider higher building heights if the applicant can demonstrate consistency with overall design guidelines intent. Special features such as towers or clerestories may be taller, if approved by the City.

Sunlight should be considered within the planned development area with regard to the height of buildings adjacent to. Generally, buildings on the south side of these spaces should be sized to allow direct year-round sunlight on south-facing structures.

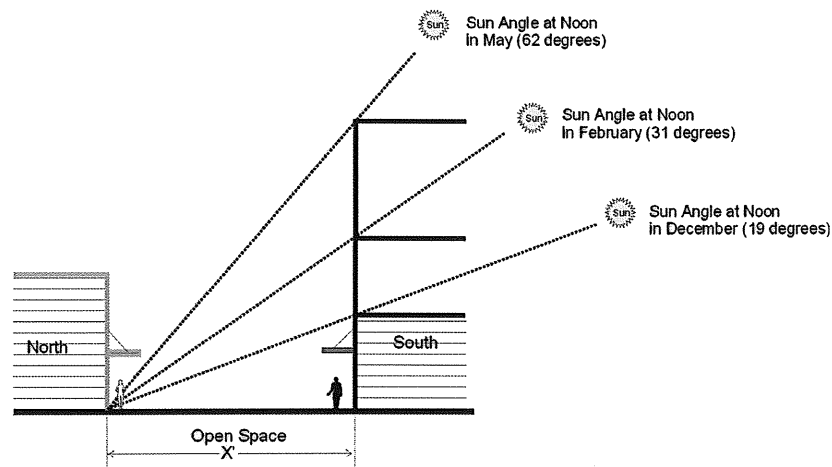


Figure 8: Sunlight should be considered in the height and design of structures adjacent to major spaces.

2. **Vertical Articulation:** In order to prevent long stretches of monotonous façade, buildings with visible facades over 100 feet in length as measured parallel to a roadway, parking area, pedestrian connection, or pedestrian area should be vertically articulated into sections averaging not more than 50 feet along the façade at regular intervals. Articulation may be accomplished in several ways, including:
 - Modulation—the stepping back or projection of a portion of the façade.
 - Including significant building elements such as balconies, porches, canopies, towers, or entry areas that visually break up the façade.
 - Building focal points that include, for example, distinctive entry features.
 - Changing the roofline.
 - Changing materials.
 - Landscaping.

- Using other methods acceptable to the City.

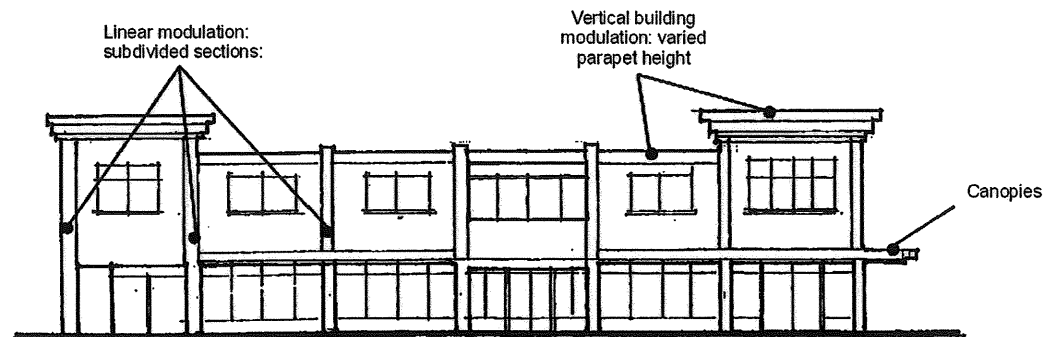


Figure 9: Building articulation: Varied parapet and recessed entries.

C. Architectural Elements

Intent:

- Create an intimately scaled, pedestrian friendly and informal architectural character.
- Reflect Monroe’s vernacular architectural character (excluding the post-War highway strip development).
- Enhance the quality of both individual buildings and the North Kelsey Planning Area streetscape as a whole.
- Encourage use of quality building materials with a low life cycle cost.
- Create design unity, a sense of place, and community identity.
- Reduce the visibility of unsightly service and utility elements from view while providing efficient service and equipment areas.

1. Roofs

Guidelines:

1. Roof designs should provide scale-reducing elements within the North Kelsey planned development area. It is recommended that buildings have a variety of roof slopes, details, materials, and configurations.
2. All flat roofs shall be architecturally treated or articulated with a parapet wall combined with ornamental molding, entablature, frieze, cornice, or other architectural roofline detail visible from the ground level. Parapets and articulated cornice lines should not appear as applied elements.
3. Roof-mounted mechanical equipment (HVAC) must be screened from view.

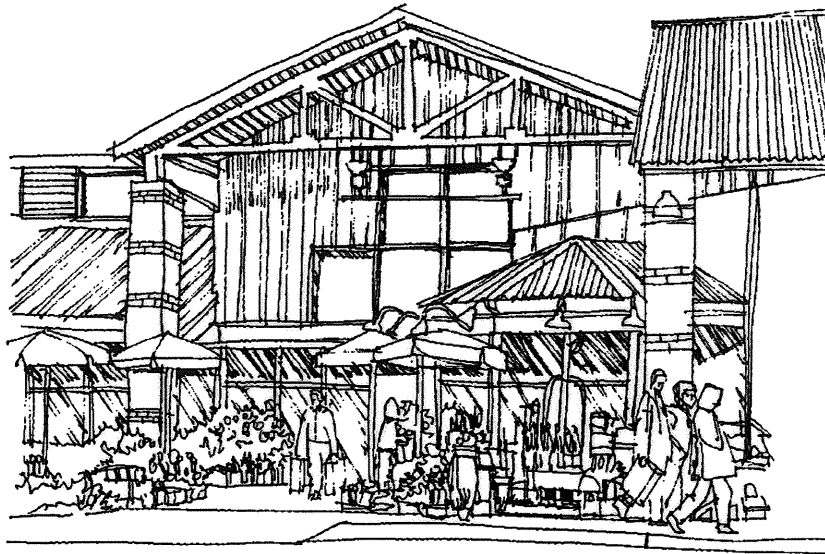


Figure 10: The gabled roof over the building entry together with pedestrian-scaled elements and a variety of building materials provides visual interest to this largely square, flat-roofed building.

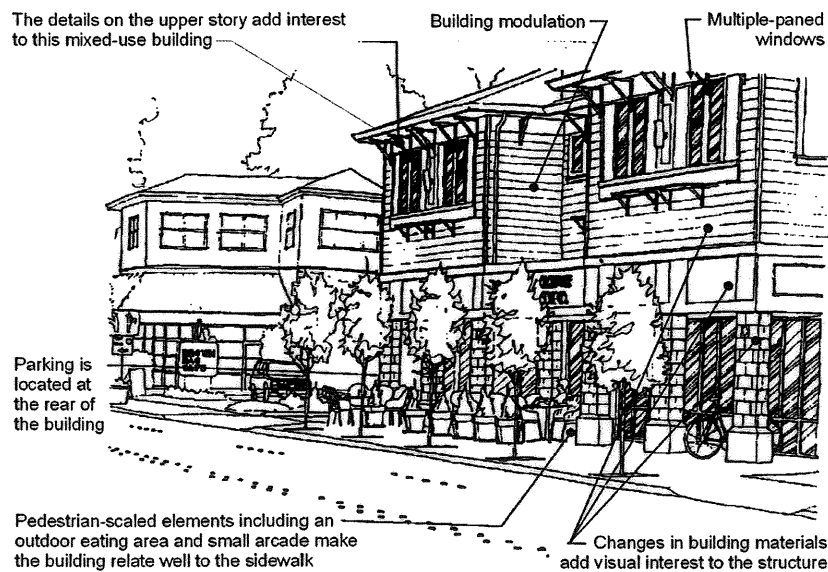


Figure 11: Examples of building details.

2. Building Elements and Details

Guideline:

1. All building facades shall incorporate a substantive use of building elements, such as those from the list that follows, as approved by the City. “Substantive” in this case means a significant contribution to the form and character of the building. Note that “decorative” means that the feature exhibits special craftsmanship or distinctive design that adds visual interest and/or unique character. Suggested building elements include:

- Articulated building elements through treatment of windows, doors, entries, and corners with special trim, molding, or glazing.
- Permanent pedestrian weather protection (building canopy).
- Decorative building materials, such as tile and metal work.
- Enhanced or articulated building entrances (recessed or covered).
- Pergolas, arcades, porches, decks, or bay windows.
- Balconies in upper stories.
- Address numbers legible to the public from the street or pathway fronting the property or building.
- Multiple-paned window fenestration (windows with several panes separated by mullions).
- Windows. All windows should either have a vertical orientation (e.g., be longer in the vertical dimension than in the width) or be square in order to qualify as special elements.

3. Exterior Materials

Guidelines:

1. Use durable and high-quality materials. Shiny or highly reflective materials are not allowed. Materials should be those of typical use in the Northwest, including:
 - Bevel or lap siding.
 - Rock, stone, and brick material.
 - Architectural shake-style roofing.
 - Metal roofs with standing seams.
2. If sheet materials, such as composite fiber products or metal siding, are used as a siding material over more than 25 percent of a building's façade, use material with a matted finish in a muted color as specified in Color guidelines below. Include the following elements:
 - Visible window and door trim painted or finished in a complementary color.
 - Corner and edge trim that covers exposed edges of the siding material.
3. If concrete blocks (concrete masonry units or "cinder blocks") are used for walls that are visible from a public street or park, use one or more of the following architectural treatments:
 - Use of textured blocks with surfaces such as split-face or grooved.
 - Use of colored mortar.
 - Use of other masonry types, such as brick, glass block, or tile, in conjunction with concrete blocks.
 - Other treatment methods approved by the City.

- The applicant shall provide the City with samples of the material, proposed detail connections and a list of other project examples in the Puget Sound region that have used this application.
4. Do not use the following materials in visible locations unless an exception is granted by the City:
 - Mirrored glass.
 - Corrugated fiberglass.
 - Chain-link fencing (with or without slats).
 - Synthetic materials with reflective surfaces, including galvanized steel and glossy vinyl siding.
 - Other treatment methods approved by the City.
 5. Paint all vents, gutters, downspouts, flashing, and electrical conduits to match the color of the adjacent surface unless they are being used expressly as a trim or accent element, or if the surface is made of an unpainted material such as brick.
 6. Provide approved address numbers so that they are legible to the public from the street fronting the property.

4. Colors

Guidelines:

1. Submit a color palette.
 2. Muted colors are encouraged for the background color of most buildings. A darker background color will allow the effective use of lighter colors for trim – where the highlights will show up better.
 3. Bright colors should generally be reserved for accents. Doors or special features may be painted a bright accent color.
 4. Bright luminescent or day-glow colors are not allowed.
- #### 5. Building Equipment and Service Areas

Guidelines:

1. Building service elements and utility equipment should be contained within the building envelope, screened from public view, or on roofs where not visible to the public.
2. All on-site service areas, loading zones, outdoor storage areas (except outdoor retail sales areas under 100 square feet in occupied area), waste storage, disposal facilities, transformer and utility vaults, and similar activities shall be located in an area not visible from a public street, pedestrian connection, or open space. If

this is not possible, then the service area, loading zone, storage area, or utility area must be screened from public view. Acceptable screening includes:

- A masonry or wood enclosure incorporated into a building wall.

- A solid hedge or other screening as approved by the City.

(Note: Visible chain link fencing with or without slats is not permitted.)

3. Service or utility areas or enclosures shall not be located in or be visible from pedestrian areas.

CHAPTER 6: Landscape Design

Intent:

- Achieve a high quality landscape that features a variety of plant materials.
- Utilize landscape materials to strengthen and unify the planning area's design identity.
- Select plant materials that are relatively hardy and require minimal maintenance.
- Add color, texture, and interest to the center.
- Screen high-impact uses.
 - A. Landscape Plan Concept

Intent:

- Provide visual relief from large expanses of parking areas and integrate new construction into the natural environment.

- Provide some physical separation between vehicular and pedestrian traffic.
- Provide decorative landscaping as a focal setting for signs, special site elements, and/or pedestrian areas.
- Provide increased areas of permeable surfaces to allow for infiltration of surface water into groundwater resources, reduce the quantity of stormwater discharge, and improve the quality of stormwater discharge.

Requirements:

1. Submit a landscape design plan and be prepared to demonstrate that the plan addresses the following considerations:
 - A unified pedestrian circulation system with amenities and plantings.
 - A coordinated system of open spaces and/or planted areas that provide the required pedestrian areas. The plan should indicate how the various spaces and plantings relate to the project's site design objectives of continuity, variety, activity, etc. The applicant should demonstrate that the landscaping treatment has a "concept" such as the example in Figure 28.
 - Screening of service or unsightly areas.
 - Plantings and/or site features that enhance the building's architectural qualities.
2. In addition, the design should consider the following landscape design objectives where appropriate:
 - Where feasible, coordinate the selection of plant material to provide a succession of blooms, seasonal color, and a variety of textures.
 - Provide a transition in landscaping design between adjacent sites, within a site, and from native vegetation areas in order to achieve greater continuity.
 - Design landscaping to create definition between public and private spaces.
 - Design landscaping to provide a transition between built structures (vertical planes) and the site (horizontal planes).
 - Use plantings to highlight significant site features and to define the function of the site, including parking, circulation, entries, and open space.

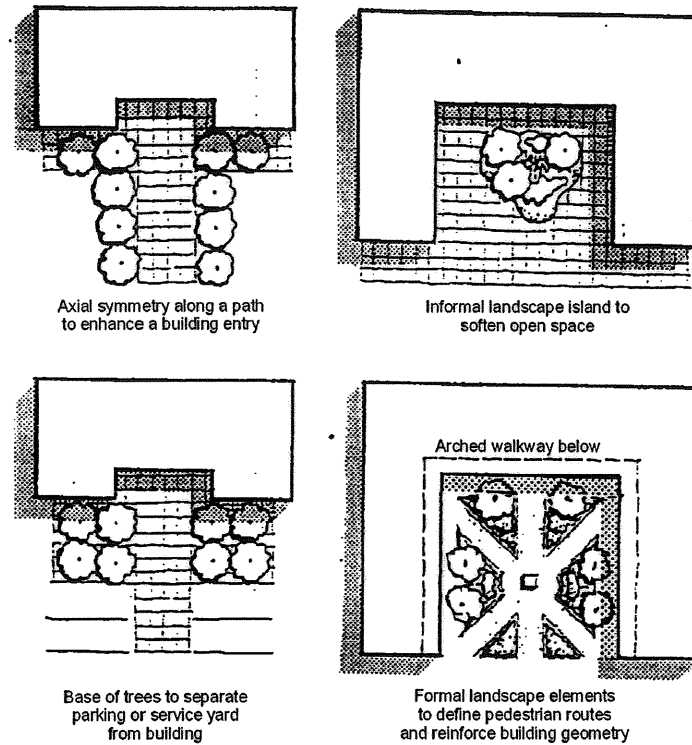


Figure 12: Examples of landscape designs associated with buildings.

B. Street Landscaping

Guidelines:

1. Sidewalks and pathways should be separated from the roadway by planting strips with street trees wherever possible.
2. Planting strips should generally be at least 5 feet in width (see Figure 8, Tjerne Place cross-section). They should include evergreen shrubs no more than 4 feet in height and/or ground cover in accordance with the City of Monroe Landscape Standards (MMC Chapter 18.78), and canopy-type broadleaf trees placed an
3. average of 25 feet on center. EXCEPTIONS: Where space is limited, planting strips less than 5 feet in width may be permitted by the City.
4. Street trees placed in tree grates may be more desirable than planting strips in key pedestrian areas.
5. Use of trees and other plantings with special qualities (e.g., spring flowers and/or good fall color) are strongly encouraged to unify development in the North Kelsey planning area.
6. Also see Building Orientation guidelines in Chapter 3 and Parking Lot Screening below for areas within the planned development area.

C. Parking Lot Screening

Guidelines:

1. Provide a landscaped drainage/stormwater treatment buffer between the sidewalk/street and parking area where possible. Size the buffer as necessary to perform required stormwater treatment function for a minimum of 20 feet is suggested See Figure 8 Otherwise, a 5-foot wide landscaping buffer consistent with Type III landscaping as specified in MMC Chapter 18.78, to provide a see-through buffer between public streets and parking lots is required; EXCEPTION: The landscape buffer must be 10 feet between parking areas and sidewalks along Chain Lake Road.

D. Parking Lot Interior

Guidelines:

1. Type IV landscaping as specified in MMC Chapter 18.78, is required to provide shade and visual relief while maintaining clear site lines within parking areas.

E. Pedestrian Area Landscaping within the Planned Development Area

These guidelines involve all other pedestrian-oriented spaces and open spaces.

Guidelines:

1. A range of landscape materials—trees, evergreen shrubs, ground covers, and seasonal flowers—shall be provided for color and visual interest.
2. Planters or large pots with small shrubs and seasonal flowers may also be used to separate café seating from traffic flow and create protected areas within the plaza for sitting and people watching.
3. Creative use of plant materials, such as climbing vines or trellises, and use of sculpture groupings or similar treatments are also encouraged.
4. All landscaping shall be as approved by the City.
5. Sun angle at noon and wind pattern should be considered in the landscaping design of the plaza to maximize sunlight areas.

F. Screening High-Impact Uses

Guidelines:

1. High impact uses such as sand and gravel mining operations or manufacturing should be screened with a landscape berm.

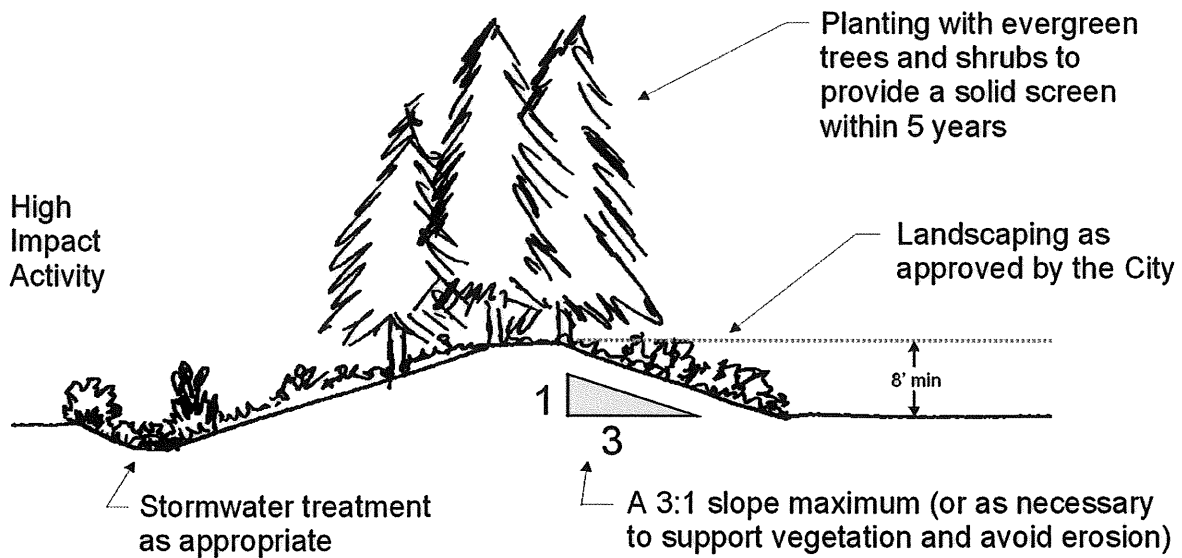


Figure 13: Screening high impact uses.

CHAPTER 7: Signage and Lighting

A. Signs

Intent:

- To encourage signage that is both clear and of appropriate scale for the project.
- To enhance the visual qualities of signage through the use of complementary sizes, shapes, colors, and methods of illumination.
- To provide signage guidelines that meet commercial tenant needs.
- To provide a comprehensive sign program that creates consistent design criteria for the entire North Kelsey planning area.

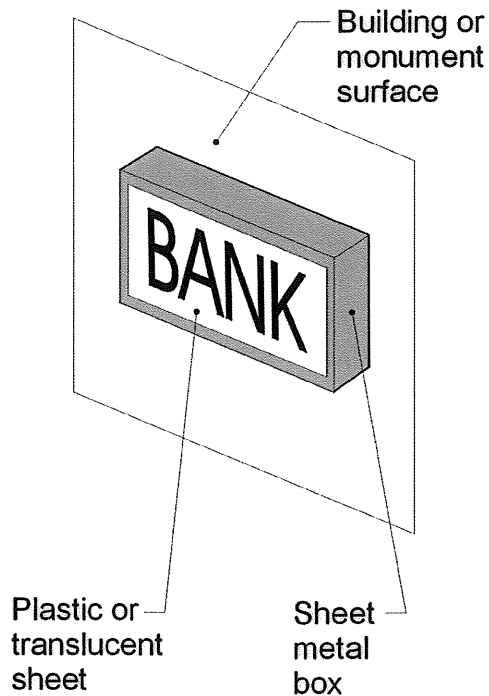
Guidelines:

1. General Standards:

Types of signage fall into three categories: monument site entry signs, tenant signs (large and small), and public and directional signs.

- a. All signage must comply with the following guidelines:
 - Signage must be of high-quality design and materials, consistent with the design of the North Kelsey planning area.
 - Signs may be fabricated of mixed media, including metal reverse-illuminated letters, suspended neon letters, illuminate individual letters, or signs etched or cut out of solid material, such as wood or brass, and illuminated from behind.
 - Signage shall be consistent throughout the planned development area and always complement a building's character (e.g., walls signs should avoid covering building columns).
- b. Non-illuminated mixed-media, and other special mixed-media signs will be subject to approval by the City.
- c. Back-lit translucent awnings and/or awning signs are not permitted.
- d. Back-lit signs with letters or graphics on a plastic sheet (can signs) are not permitted. Generally, these signs are of low quality, easily broken, and not integrated into the building's architecture. However, back-lit letters or graphics that are part of a sign that is integrated into the building's architecture are permitted (see examples on the following pages).

**Typical “can signs”
are not acceptable**



**Internally lit letters
or graphics are acceptable**

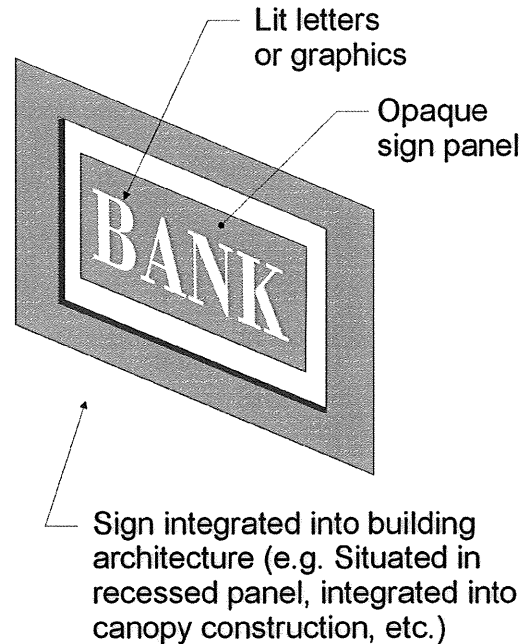


Figure 30: Appropriate and inappropriate signage.

- e. The following signs and sign elements are prohibited:
- Pole-mounted signs.
 - Signs employing moving or flashing lights.
 - Signs employing exposed electrical conduits.
 - Visible ballast boxes or other equipment.
 - Signs with luminous plastic letters.
 - Audible or odor-producing signs.
 - Cardboard signs.
 - Roof-mounted and temporary-type signage, such as banners. (Note: Temporary signage for special occasions will be permitted, subject to City approval of both design and duration of display.)
 - Off-premises signs, except public directional and site identification signs.
- f. Cinemas and community centers may have changeable letter signage.

2. Public and Directional Signs:

a. *Placement:*

- (1) Public and directional signs may include directional signage and street name markers, pedestrian trail markers, project tenant directories, kiosks, theme elements, and miscellaneous exterior site signage. The design of these elements typically shall share consistent design patterns and express character of the North Kelsey planning project.
- (2) Directional signage shall be required to direct traffic to public parking.
- (3) Pedestrian trail/routes shall be identified.
- (4) Public and directional signs of the North Kelsey planned development area shall be coordinated throughout the site. Information and directional signage design must integrate with the various design areas.

b. *Maximum Size and Quantity Limitations:*

- (1) Parking lot signage for the planned development area shall be limited to one sign per entrance.
- (2) The size and areas of public directional signs shall be reviewed and approved by the City based on design quality, attractiveness and thematic, functional, and scale appropriateness.

3. Tenant Signs:

For purposes of these guidelines, 'Tenants' shall be considered those with less than 12,000 square feet of leasable area, and "Large Tenants" shall have a leasable area of 12,000 square feet and greater.

a. *Placement:*

- (1) Wall signs for retail shops shall be located in the storefront area above door height and below the canopy (typically 8 feet above the floor). Wall signage located at building focal points is an exception to this guideline.

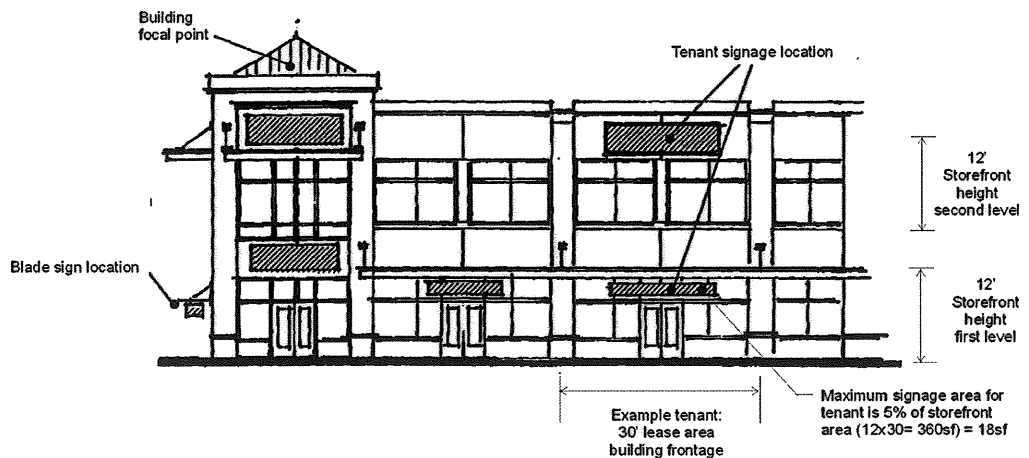


Figure 30: Sign design standards.

- (2) Signs shall be centered between architectural elements and between columns to allow building architecture to be expressed. Signs shall not necessarily be centered on leased premises.
- (3) Signs shall be compatible in scale and proportion with building design and other signs.
- (4) Blade signs, which hang from the canopy, arcade, or building front, may be utilized to increase visibility.
- (5) Upper-level tenants and large tenants may have signs located above the storefronts and below roof-line or cornices, subject to City approval, to accommodate signs visible from adjacent streets.
- (6) Service/secondary signage shall be allowed.
- (7) Signage within the planned development area shall be placed facing public plazas and/or streets. When tenants face two streets or a street and a plaza, signage is allowed on both facades. Each sign is calculated separately and shall conform to all applicable maximum area limitations. Calculated maximum areas are not transferable to other facades. Each tenant is allowed to place signage on no more than two facades.

b. *Size/Design:*

- (1) Office tenant signage maximum is 100 square feet. Approval shall be based on sign design quality, attractiveness, scale appropriateness, and compatibility with the building to which it is attached.
- (2) Letter height shall not exceed 18 inches. Larger first letters up to 24 inches will be permitted.
- (3) Length of the signs shall not be more than two-thirds of the overall leased façade area or less than 3 feet from the demising wall of the leased premises.
- (4) Wall-mounted signs shall not project more than 6 inches from the building.
- (5) Hanging blade signs shall not have an area of more than 3 square feet or exceed 2 inches in thickness. Blade sign area is not included in the signage area. The bottom of the sign should not be lower than 8 feet above ground.
- (6) Service/secondary signage maximum area is 2 square feet.

Tenant Signage Requirements within the Planned Development Area:

- Tenant signage calculations apply to tenants with *less* than 12,000 square feet of leasable area.
- Maximum signage area is calculated at 5 percent of storefront area.
- Maximum signage area shall not exceed 24 square feet in total area for any single storefront.
- Each street or plaza frontage is calculated separately.
- Maximum area calculation shall not transfer to other storefronts on other building elevations.

- “Storefront” refers to the first 12 feet above each tenant floor of lease area building frontage facing a street or plaza.

Example: Storefront area dimensions: 12' x 30' = 360 SF.

Signage allowed: 360 SF x 5% = 18 SF

Large tenant signage requirements:

- Tenant signage calculations apply to tenants with *more* than 12,000 square feet of leasable area.
- Maximum signage area is calculated at 5 percent of storefront area.
- Maximum signage area shall not exceed 200 square feet in total area for any single storefront.
- Each street or plaza frontage is calculated separately.
- Maximum area calculation shall not transfer to other storefronts on other building elevations.
- “Storefront” refers to the first 12 feet above each tenant floor of lease area building frontage facing a street or plaza.

Example: Storefront area dimensions: 12' x 90' x 2 floors = 2,160 SF.

Signage allowed: 2,160 SF x 5% = 108 SF

4. Monument Site Entry Signs for the Planned Development Area:

a. *Placement and Design:*

- (1) Monument Site Entry Signs may be located at major entries connecting to SR-2 and other streets connecting to the planned development area (also see Street Corners/Highly Visible Locations, Chapter 3).
- (2) Signs shall be designed to integrate with adjacent site landscaping.
- (3) Entry signs shall be unified by common design and graphic elements. It is encouraged that monument entry signs share design elements with the public and directional signs throughout the site.
- (4) Entry signs shall identify the North Kelsey planned development. No individual tenant names shall be identified.

b. *Maximum Size and Quantity Limitations:*

- (1) The maximum height and width shall be as approved by the City, with proposals reviewed for scale, design quality, attractiveness, and functional appropriateness for the North Kelsey planning area.

B. Lighting

Intent:

- To provide adequate lighting to ensure safety and security.

- To enhance and encourage evening activities.
- To provide a distinctive character to the area.

Standards:

1. Sidewalks must average between 1 and 2 foot-candles. Primary Pathways must average between 2 and 4 foot-candles. The Focal Plaza, Shopping Corridor, and Pedestrian-Oriented Spaces within the Planned Development Area must be at least 2 foot-candles (also see Chapter 3). Parking areas must average at least 1 foot-candle. Parking lot pathways must be average at least 2 foot-candles.
2. The color of light should be considered in the lighting design. For example, metal halide is recommended for general usage at building exteriors, parking areas, and pedestrian courts as well as in streetlamps and for lighting street trees. Low-pressure sodium, which casts a yellow light, is not recommended except for streets and parking lots.
3. Uplighting on trees and provisions for seasonal lighting are encouraged.
4. All efforts to reduce glare into the North Kelsey planning area and surrounding community from street and parking area lights should be undertaken.
5. Accent lighting on architectural and landscape features is encouraged to add interest and focal points.
6. Pedestrian-scaled lighting is required within the planned development area along all streets and in all public plazas and courts. Pedestrian-scaled lighting fixtures are generally 12-14 feet and of a character complementary to the building architecture.
7. Parking area lighting shall not exceed 25 feet in height and shall be baffled to minimize glare and spillage into the surrounding community.

Definitions

Articulation – Articulation is design emphasis placed on a particular architectural feature by special details, materials, change in building plane (recessed or extended from building surface), contrast in materials, or decorative artwork.

Blank Walls - Walls subject to "blank wall" requirements meet the following criteria:

- Any wall or portion of a wall that has a surface area of 400 SF of vertical surface without a window, door, building modulation as defined below or other architectural feature (see figure below for measuring methods).
- Any ground level wall surface or section of a wall over 4' in height at ground level that is longer than 15' as measured horizontally without having a ground level window or door lying wholly or in part within that 15' section (see below).

Human Scale - The term "human scale" refers to the perceived size of a building relative to a human being. A building is considered to have "good human scale" if there is an expression of human activity or use that indicates the building's size. For example, traditionally sized doors, windows, and balconies are elements that respond to the size of the human body, so these elements in a building indicate a building's overall size.

Modulation - In the design guidelines, modulation is a stepping back or projecting forward of portions of a building face within specified intervals of building width and depth, as a means of breaking up the apparent bulk of a structure's continuous exterior walls.

Pathways – Refer to any pedestrian route other than a *sidewalk*.

Pedestrian-Oriented - Can be small to large widening of walking space, landscaped areas, areas for outdoor dining, or small play areas (see guidelines and requirements for *Pedestrian-Oriented Spaces* in Chapter 3).

Sidewalks - Refer to concrete pedestrian routes adjacent to public right-of-ways.

Weather Protection – Architectural features such as an awning, marquee, or canopy that protects pedestrians from rain and sunlight.